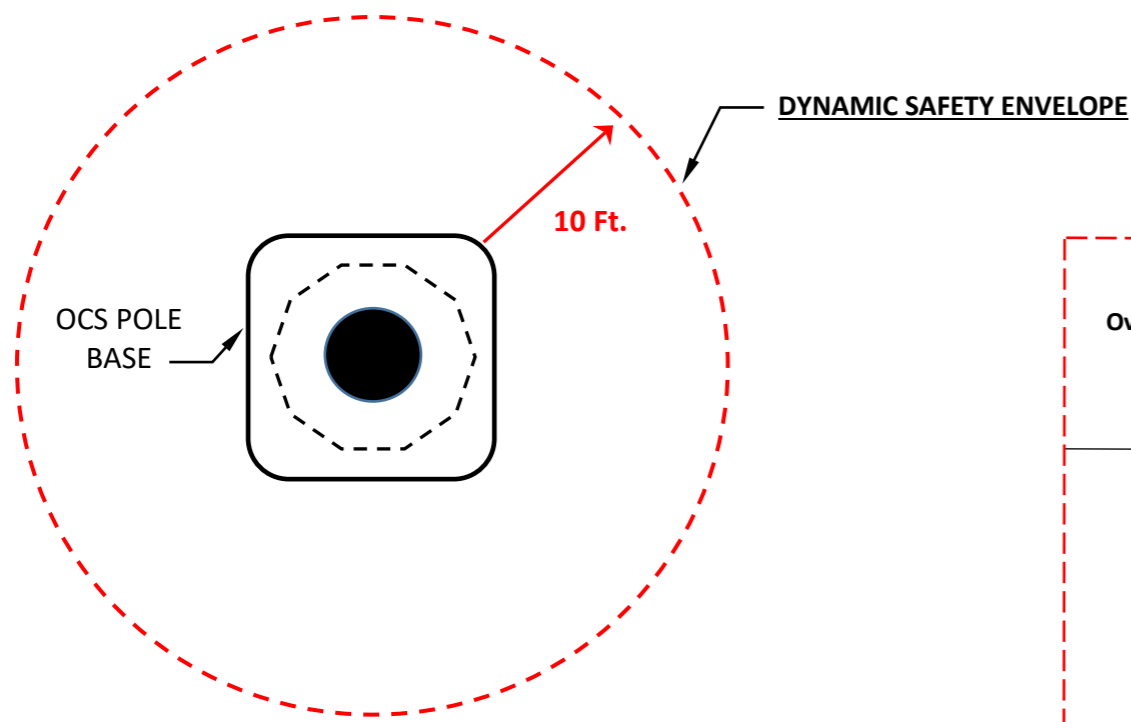
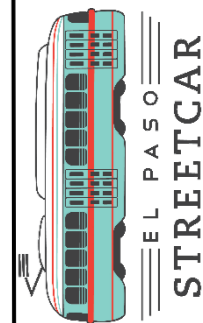


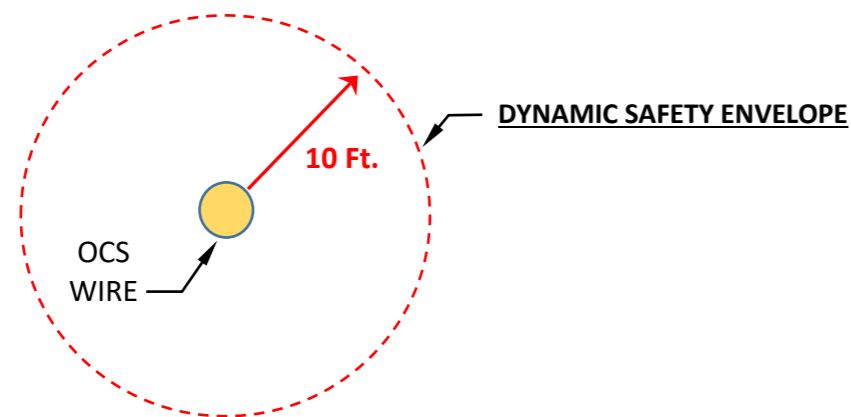
Exhibit C

Operational Right-of-Way / Dynamic Safety Envelope





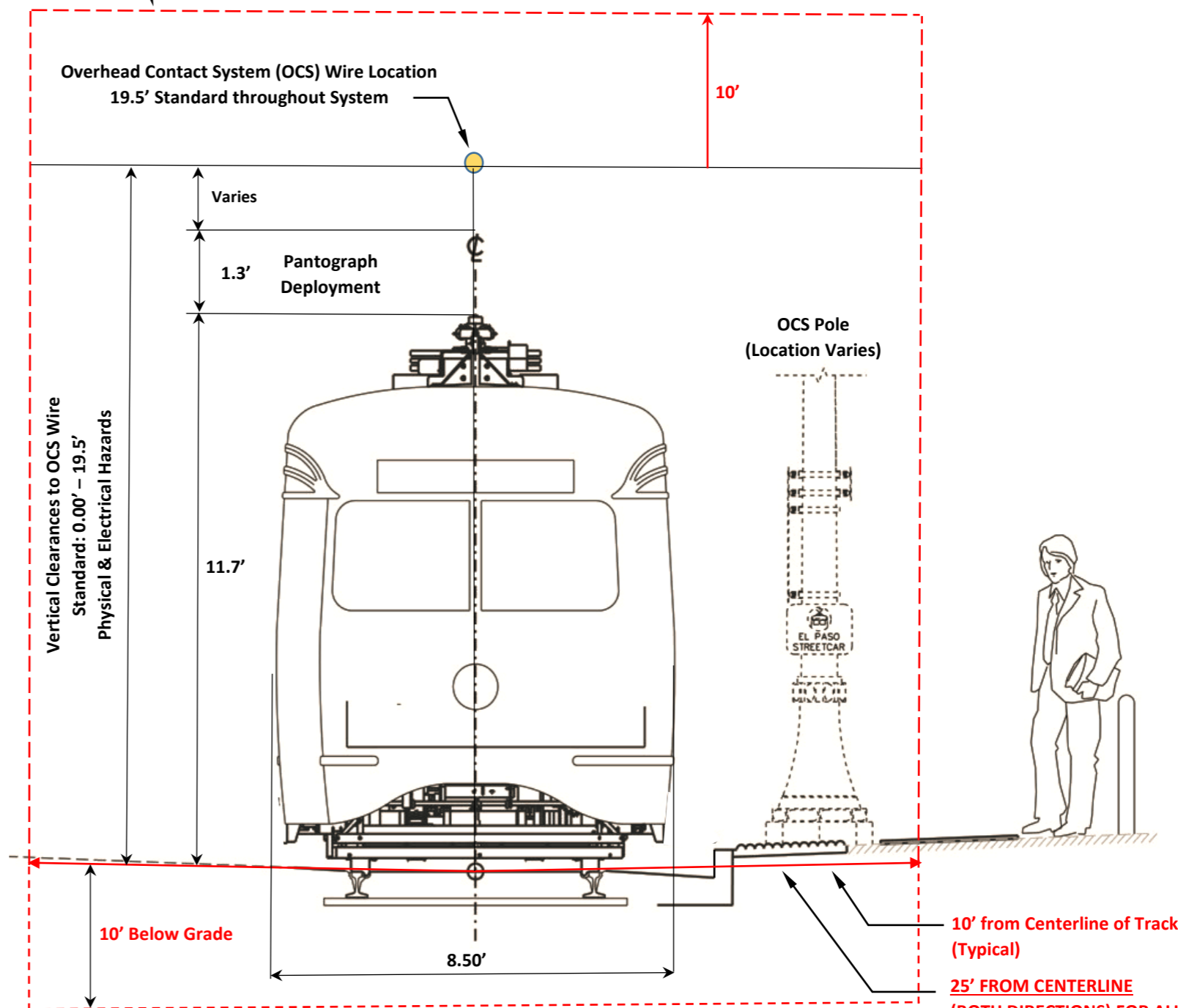
TYPICAL PLAN VIEW – OVERHEAD CONTACT SYSTEM (OCS) POLE – SCALE: N.T.S.



TYPICAL SECTION VIEW – OVERHEAD CONTACT SYSTEM (OCS) WIRE – SCALE: N.T.S.

GENERAL NOTES:

1. THE DYNAMIC SAFETY ENVELOPE (DSE) EXTENDS 10 FEET Laterally FROM THE CENTERLINE OF THE TRACK, 10 FEET SURROUNDING THE OCS WIRE, AND 10 FEET SURROUNDING ANY OTHER STREETCAR INFRASTRUCTURE, INCLUDING OCS POLES AND APPURTENANCES.
2. THE DSE ALSO EXTENDS 10 FEET BELOW GRADE FROM THE TOP OF THE FINISHED TRACK ELEVATION.
3. THE STREETCAR ZONE OF INFLUENCE EXTENDS 25 FEET FROM THE CENTERLINE OF THE TRACK FOR ALL EXCAVATIONS.
4. PHYSICAL AND ELECTRICAL HAZARDS EXIST ANYWHERE INSIDE THE DYNAMIC SAFETY ENVELOPE.
5. COORDINATION WITH EL PASO STREETCAR IS REQUIRED FOR ANY WORK INSIDE THE DSE.



TYPICAL SECTION VIEW – OPERATIONAL RIGHT-OF-WAY / DYNAMIC SAFETY ENVELOPE – SCALE: N.T.S.